

CTR Employer Survey Report

Thank you for completing your Commute Trip Reduction survey. This report contains the survey results.

Employer Id : E84772

Employer : The Boeing Company

Worksite : The Boeing Company - Renton Village

Street : 700 S Renton Village Pl Bldg #2

Jurisdiction : City of Renton

Survey Type : Online

Survey Date : 5/8/2016

Response Rate : 76%

Drive Alone & One-Way VMT Rates at this Worksite

Employees and Survey Response Information

Drive Alone : 83.0%
One-Way VMT per employee : 16.0

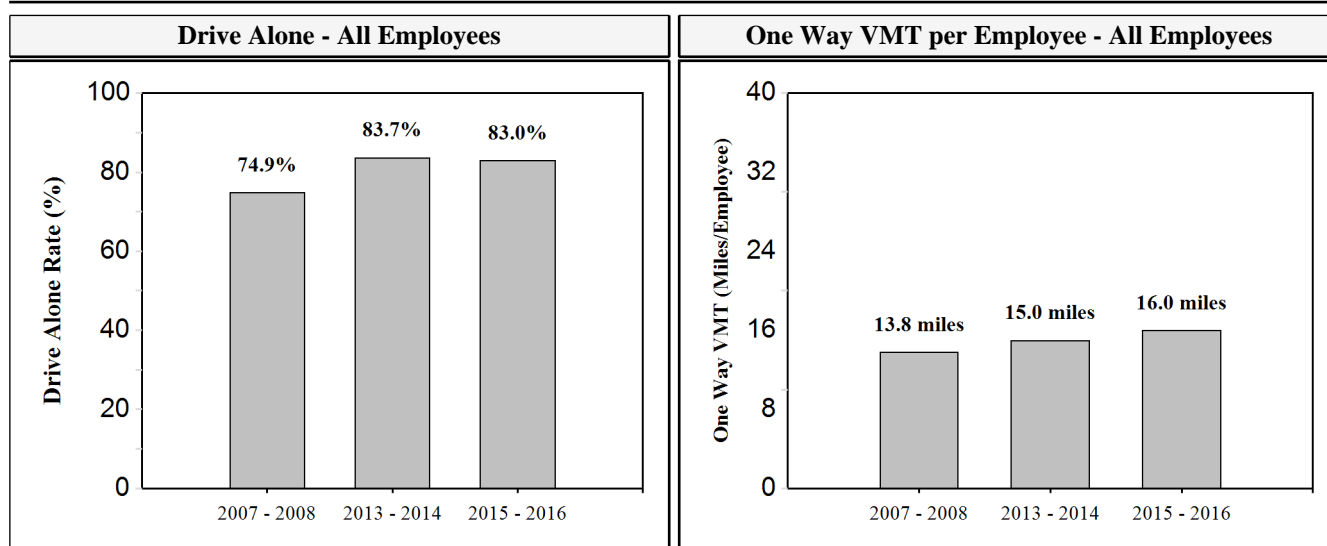
Reported Total Employees at Worksite: 835

Surveys Distributed : 835

Surveys Returned : 635

Surveys Returned by CTR Affected Employees : 617

Total Estimated CTR - Affected Employees at Worksite : 811



Site History and Goal

Cycle	Drive Alone - All	Drive Alone - CTR Affected	VMT / Employee - All	VMT / Employee - CTR Affected
2007 - 2008	74.9%	74.6%	13.8	13.3
2009 - 2010	73.7%	73.7%	13.9	13.9
2011 - 2012	85.4%	85.5%	15.6	15.4
2013 - 2014	83.7%	83.7%	15.0	14.9
2015 - 2016	83.0%	82.6%	16.0	15.9
2017 - 2018	N/A	N/A	N/A	N/A
2019 - 2020	N/A	N/A	N/A	N/A
Goal	TBD	TBD	TBD	TBD
Percent Change	10.8%	10.7%	15.9%	19.5%

Comparison Between Rates With and Without Fill-In

The survey response rate is indicated on Page 1. To encourage a response rate of at least 70%, additional drive alone trips are added to survey results for worksites with a response rate of less than 70%. For these worksites it is assumed that non-responding employees between the actual response rate and 70% drive alone 5 days a week. These additional trips represent the "Fill-In" applied. Note that fill-in is not applied to a worksite's first survey in the 2007 to 2012 cycle (their baseline survey).

	2007 - 2008	2013 - 2014	2013 - 2014 Without Fill In	2015 - 2016
Drive Alone - All Employees*	74.9%	83.7%	83.4%	83.0%
Drive Alone - CTR Affected Employees*	74.6%	83.7%	83.4%	82.6%
VMT/Employee - All Employees	13.8	15.0	15.0	16.0
VMT/Employees - CTR Affected Employees	13.3	14.9	14.9	15.9

* Drive alone rate includes one person motorcycles.

Congratulations! You achieved a survey response rate of 70% or higher on this survey. Fill-in comparison for previous surveys, if applicable, are included in the chart above.

GHG Emissions: Total for Drive Alone, Carpools, Vanpools

Annual Greenhouse Gas Emissions (Metric Tons CO₂e) for Roundtrip Commute*

Value	2007 - 2008	2013 - 2014	2015 - 2016
Emissions for Surveyed Employees	3,713	2,137	2,073
Estimated Emissions for Total Employment	4,618	3,104	2,725

* Estimated based on VMT from commuters driving alone, carpooling, vanpooling, or motorcycling, without fill-in applied.

Bus Transit Passenger Miles and Rail Transit Passenger Miles*

Annual Transit Passenger Miles (includes Roundtrip Commute)	2007 - 2008	2013 - 2014	2015 - 2016
Bus Annual Passenger Miles - Estimated for Total Employment	149,500	72,492	152,404
Bus Annual Passenger Miles - Surveyed Employees	120,200	49,900	115,900
Ferry Annual Passenger Miles - Estimated for Total Employment	0	68,715	100,989
Ferry Annual Passenger Miles - Surveyed Employees	0	47,300	76,800
Train/Light Rail/Streetcar Annual Passenger Miles - Estimated for Total Employment	104,600	265,126	193,299
Train/Light Rail/Streetcar Annual Passenger Miles - Surveyed Employees	84,100	182,500	147,000

* Transit passenger miles can be used to gauge changes in transit usage, and also to calculate greenhouse gas emissions from transit commute trips. However, emissions attributable to transit vary widely, depending on the efficiency/energy source of transit vehicles and transit vehicle passenger load (typically ranging from 0.1 to 0.9 pounds CO₂e emissions/passenger mile). Employers are strongly encouraged to contact their local transit agencies for more precise information on GHG emissions for their transit trips. If nothing else is available, the value of 0.47 pounds (0.00021 metric tons) per passenger mile can be used to estimate CO₂e emissions for bus transit, and 0.39 pounds (0.00018 metric tons) CO₂e emissions per passenger mile for train/light rail/streetcar.

Q3.

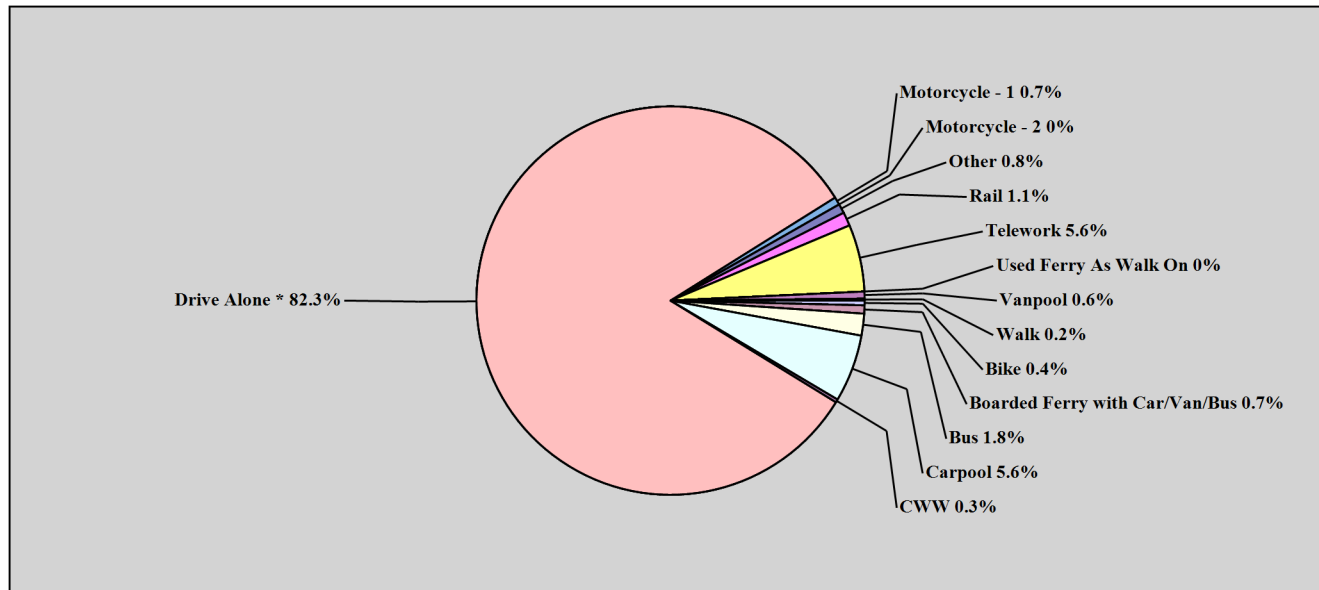
One way, how many miles do you commute from home to your usual work location?

Average one-way distance home to work: 18.8 miles

Commute Trips By Mode - All Employees

Q.4a: Last week, what type of transportation did you use each day to commute TO your usual work location? (Mode used for the longest distance.)

Mode Split - All Employees



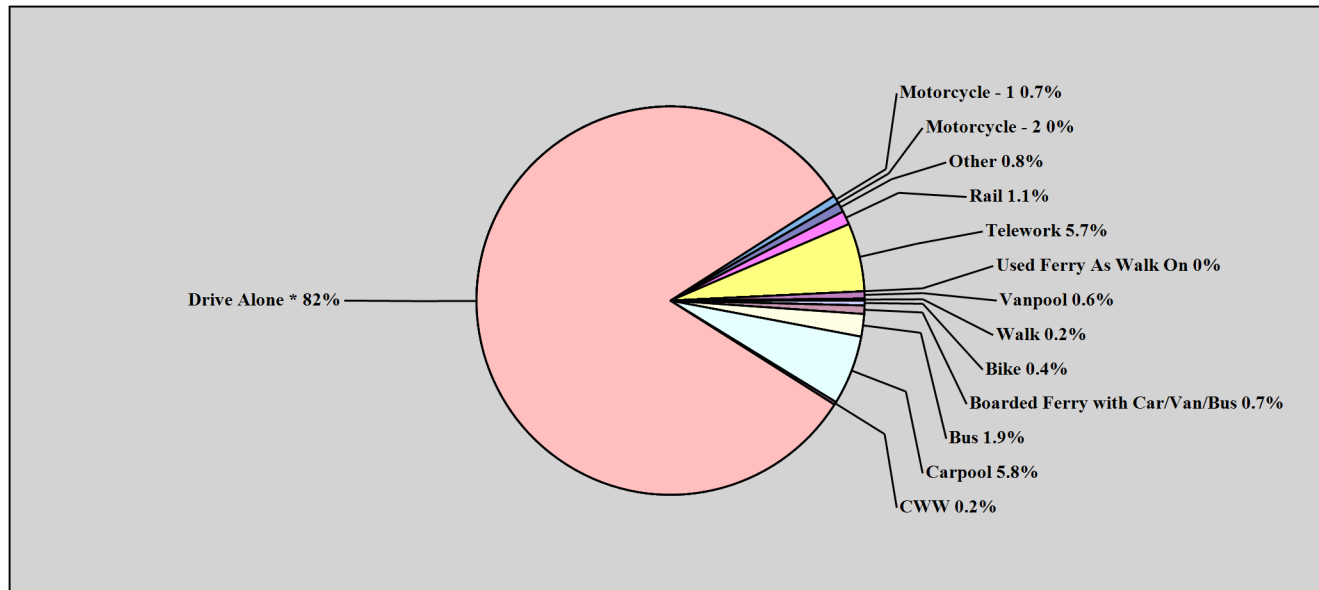
Mode	Trips During This Survey Week	% of Trips During This Survey Week	% of Trips During Previous Survey Week	Employees Who Used This Mode at Least Once During This Survey Week	% of Employees Who Used This Mode at Least Once During This Survey Week	% of Employees Who Used This Mode at Least Once During Previous Survey Week
Drive Alone *	2,535	82.3%	83.3%	564	88.8%	91.0%
Carpool	173	5.6%	5.0%	48	7.6%	7.0%
Vanpool	18	0.6%	0.6%	5	0.8%	0.9%
Motorcycle - 1	20	0.7%	0.3%	7	1.1%	0.6%
Motorcycle - 2	0	0.0%	0.0%	0	0.0%	0.2%
Bus	56	1.8%	0.8%	17	2.7%	1.4%
Rail	35	1.1%	1.6%	10	1.6%	2.0%
Bike	12	0.4%	0.2%	6	0.9%	0.6%
Walk	5	0.2%	0.1%	2	0.3%	0.2%
Telework	172	5.6%	6.4%	125	19.7%	18.4%
CWW	8	0.3%	0.4%	8	1.3%	1.7%
Boarded Ferry with Car/Van/Bus	21	0.7%	0.4%	5	0.8%	0.6%
Used Ferry As Walk On	0	0.0%	0.0%	0	0.0%	0.0%
Other	24	0.8%	0.9%	10	1.6%	1.7%

* Drive alone mode includes fill-in, where applicable.

Commute Trips By Mode - Affected Employees

Q.4a: Last week, what type of transportation did you use each day to commute TO your usual work location? (Mode used for the longest distance.)

Mode Split - Affected Employees



Mode	Trips During This Survey Week	% of Trips During This Survey Week	% of Trips During Previous Survey Week	Employees Who Used This Mode at Least Once During This Survey Week	% of Employees Who Used This Mode at Least Once During This Survey Week	% of Employees Who Used This Mode at Least Once During Previous Survey Week
Drive Alone *	2,452	82.0%	83.3%	546	88.5%	91.3%
Carpool	173	5.8%	4.8%	48	7.8%	6.9%
Vanpool	18	0.6%	0.6%	5	0.8%	0.9%
Motorcycle - 1	20	0.7%	0.3%	7	1.1%	0.6%
Motorcycle - 2	0	0.0%	0.0%	0	0.0%	0.2%
Bus	56	1.9%	0.8%	17	2.8%	1.4%
Rail	34	1.1%	1.7%	9	1.5%	2.0%
Bike	12	0.4%	0.2%	6	1.0%	0.6%
Walk	5	0.2%	0.1%	2	0.3%	0.2%
Telework	170	5.7%	6.5%	123	19.9%	18.6%
CWW	7	0.2%	0.3%	7	1.1%	1.6%
Boarded Ferry with Car/Van/Bus	21	0.7%	0.4%	5	0.8%	0.6%
Used Ferry As Walk On	0	0.0%	0.0%	0	0.0%	0.0%
Other	24	0.8%	0.9%	10	1.6%	1.7%

* Drive alone mode includes fill-in, where applicable.

Alternative Modes - Number of Employees Who Used a Non-Drive Alone Mode:

Non-Drive Alone Number Of Days	Exactly this # of Employees	Exactly this % of Employees	At least # of Employees	At least % of employees
0 Day	428	67%	635	100%
1 Days	86	14%	207	33%
2 Days	40	6%	121	19%
3 Days	17	3%	81	13%
4 Days	18	3%	64	10%
5 Days	42	7%	46	7%
6 or More Days	4	1%	4	1%

Work Schedules By Group - All Employees (This table shows the relationship between work schedule and commute mode)

Employees who worked:	Drive Alone 5 days / week		Drive Alone 3 or 4 days / week		Used Bus At Least 3 days / week		Carpooled At Least 3 days / week		Used Rail At Least 3 days / week		Vanpooled At Least 3 times / week		Biked or Walked At Least 3 Days / week		Used 'Other' Modes At Least 3 Days / week		Used Non-Drive Alone At Least 3 Days / week	
5 days a week	358	60.4%	134	22.6%	11	1.9%	33	5.6%	7	1.2%	5	0.8%	3	0.5%	2	0.3%	74	12.5%
4 days a week (4/10s)	3	37.5%	5	62.5%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%
3 days a week	0	0%	1	100%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%
9 days in 2 weeks (9/80)	9	32.1%	12	42.9%	0	0%	3	10.7%	0	0%	0	0%	0	0%	0	0%	6	21.4%
7 days in 2 weeks	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%	0	0%
Other	0	0%	3	75%	0	0%	0	0%	1	25%	0	0%	0	0%	0	0%	1	25%

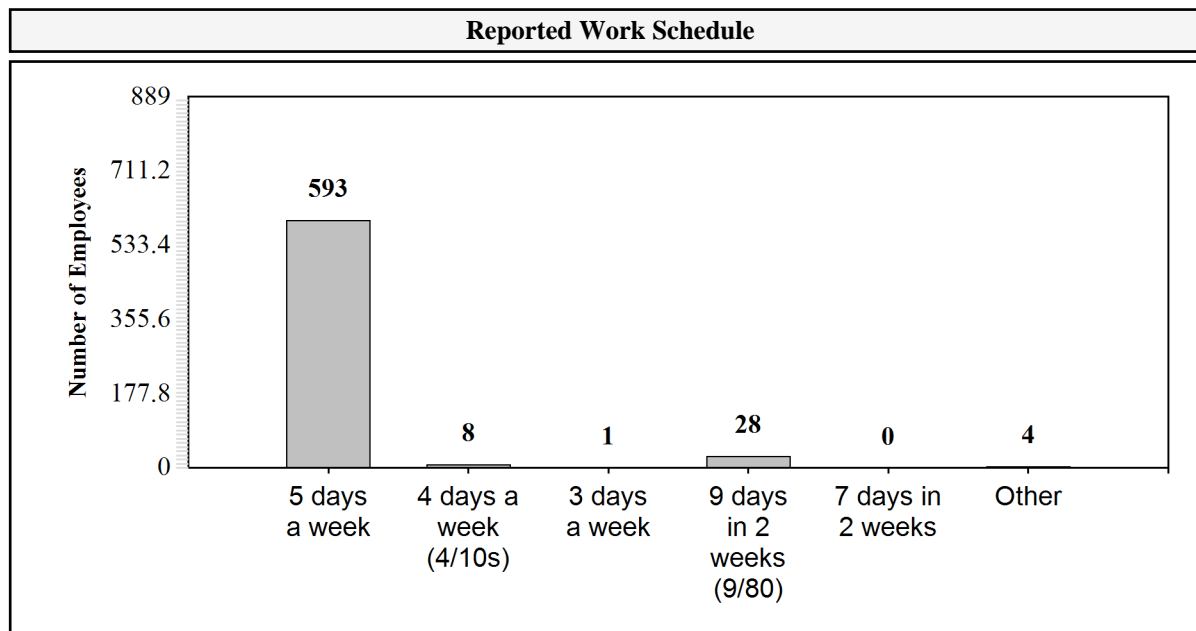
Count by Occupancy of Carpools, Vanpools, and Motorcycles

Q.4b If you used a carpool or vanpool as part of your commute, or if you ride a motorcycle, how many people (age 16 or older) are usually in the vehicle?

Ridesharing Occupancy	Mode	Response Count
1	Motorcycle	21
2	Motorcycle	0
2	Carpool	146
3	Carpool	13
4	Carpool	14
5	Carpool	0
>5	Carpool	0
<5	Vanpool	4
5	Vanpool	8
6	Vanpool	3
7	Vanpool	0
8	Vanpool	3
9	Vanpool	0
10	Vanpool	0
11	Vanpool	0
12	Vanpool	0
13	Vanpool	0
14	Vanpool	0
15	Vanpool	0

Reported Work Schedule - All Employees

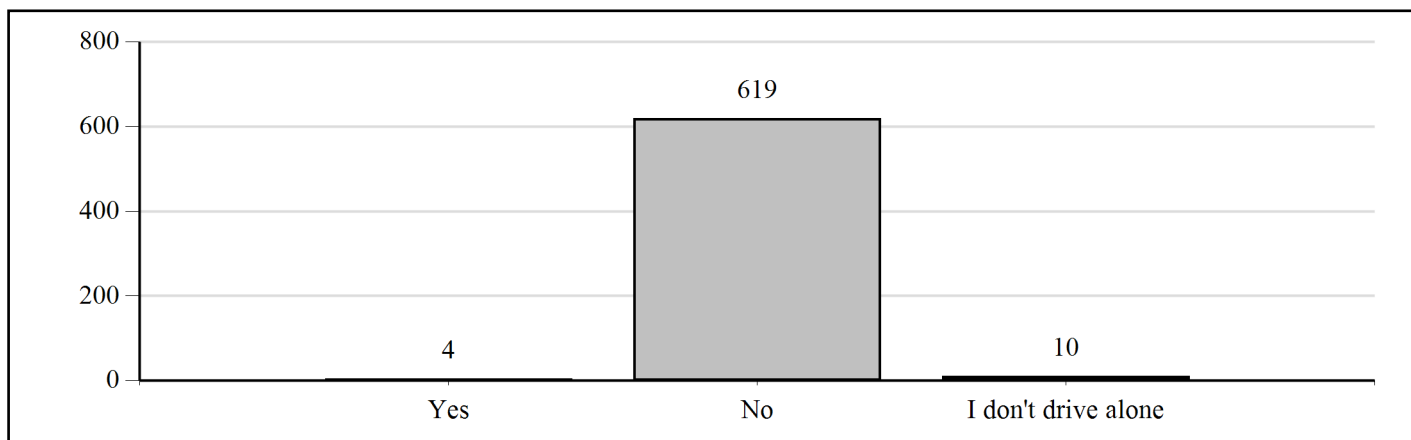
Q.5 Which of the following best describes your work schedule?



Reported Work Schedule	# Of Responses	% Of Employees
5 days a week	593	93.5%
4 days a week (4/10s)	8	1.3%
3 days a week	1	0.2%
9 days in 2 weeks (9/80)	28	4.4%
7 days in 2 weeks	0	0%
Other	4	0.6%

Parking and Telework

Q.9: On the most recent day that you drove alone to work, did you pay to park? (Mark "yes" if you paid that day, if you prepaid, if you are billed later, or if the cost of parking is deducted from your paycheck.)



Q.10: How many days do you typically telework?

Telework Frequency	# of Responses	% of Responses
No Answer/Blank	1	0.2%
I don't telework	211	33.2%
Occasionally, on an as-needed basis	257	40.5%
1-2 days/month	38	6.0%
1 day/week	97	15.3%
2 days/week	26	4.1%
3 days/week	5	0.8%

Reasons for driving alone to work/not driving alone to work

Q11. When you do not drive alone to work, what are the three most important reasons?

Question Text	# of Responses	% of Responses
I have the option of teleworking	237	19.4%
To save time using the HOV lane	176	14.4%
To save money	163	13.3%
Personal health or well-being	132	10.8%
Other	105	8.6%
Environmental and community benefits	104	8.5%
Free or subsidized bus, train, vanpool pass or fare benefit	76	6.2%
Financial incentives for carpooling, bicycling or walking.	69	5.6%
Driving myself is not an option	57	4.7%
Cost of parking or lack of parking	50	4.1%
Emergency ride home is provided	35	2.9%
Preferred/reserved carpool/vanpool parking is provided	13	1.1%
I receive a financial incentive for giving up my parking space	6	0.5%

Q12. When you drive alone to work, what are the three most important reasons?

Question Text	# of Responses	% of Responses
Riding the bus or train is inconvenient or takes too long	458	28.5%
I like the convenience of having my car	413	25.7%
Family care or similar obligations	209	13.0%
Other	138	8.6%
My job requires me to use my car for work	137	8.5%
My commute distance is too short	131	8.2%
Bicycling or walking isn't safe	74	4.6%
I need more information on alternative modes	37	2.3%
There isn't any secure or covered bicycle parking	8	0.5%

Employee Transit Use - All Employees

Q 13. Please indicate the number of one-way transit or walk-on ferry trips you took last week on each system listed below (for any purpose, not just getting to and from work). Please select "Other" if your transit isn't listed.

Trips/Week	Employees Making This Many Transit Trips in a Week									
	Community Transit	Everett Transit	Intercity Transit	King County Metro	Kitsap Transit	Pierce Transit	Sound Transit	Whatcom Transportation Authority	Ferry as Walk-On	Other
1	2	1	0	14	0	1	8	0	5	3
2	4	0	0	9	1	0	8	0	3	1
3	0	0	0	1	0	0	1	0	0	0
4	1	0	0	5	0	0	1	0	0	2
5	0	0	0	4	0	0	3	0	0	2
6	0	0	0	2	0	0	3	1	0	0
7	0	0	0	0	0	0	0	0	0	0
8	2	0	0	3	0	1	2	0	0	0
9	0	0	0	0	0	0	0	0	0	0
10	1	0	0	3	0	0	4	0	0	0
11 or more	0	0	0	3	0	0	0	0	0	0
# Of Employees using Transit	10	1	0	44	1	2	30	1	8	8
Total One-Way Transit Trips Per Week	40	1	0	197	2	9	120	6	11	23

Employee Transit Use - Affected Employees

Q 13. Please indicate the number of one-way transit or walk-on ferry trips you took last week on each system listed below (for any purpose, not just getting to and from work). Please select "Other" if your transit isn't listed.

Trips/Week	Employees Making This Many Transit Trips in a Week									
	Community Transit	Everett Transit	Intercity Transit	King County Metro	Kitsap Transit	Pierce Transit	Sound Transit	Whatcom Transportation Authority	Ferry as Walk-On	Other
1	2	1	0	13	0	1	8	0	5	2
2	4	0	0	9	1	0	8	0	3	1
3	0	0	0	1	0	0	1	0	0	0
4	1	0	0	5	0	0	1	0	0	2
5	0	0	0	4	0	0	3	0	0	2
6	0	0	0	2	0	0	3	1	0	0
7	0	0	0	0	0	0	0	0	0	0
8	2	0	0	3	0	1	2	0	0	0
9	0	0	0	0	0	0	0	0	0	0
10	1	0	0	3	0	0	4	0	0	0
11 or more	0	0	0	3	0	0	0	0	0	0
# Of Employees using Transit	10	1	0	43	1	2	30	1	8	7
Total One-Way Transit Trips Per Week	40	1	0	196	2	9	120	6	11	22

Commute Mode By ZipCode for All Employees

Q8. What is your home zip code?

Home Zip code	Total Employees	Employee Percentage	Weekly Count of Trips By Mode												
			Drive Alone	Carpool	Vanpool	Motorcycle	Bus	Train	Bike	Walk	Telework	CWW	Ferry (Car/Van/Bus)	Ferry (walk-on)	Other
	1	0.16%	3	0	0	0	0	0	0	0	0	0	0	0	0
68058	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98001	10	1.57%	40	2	0	0	0	0	2	0	3	1	0	0	0
98002	3	0.47%	9	5	0	0	0	0	0	0	0	0	0	0	0
98003	6	0.94%	21	6	0	0	0	0	0	0	0	0	0	0	0
98004	7	1.10%	25	9	0	0	0	0	0	0	0	0	0	0	0
98005	2	0.31%	9	0	0	0	0	0	0	0	0	0	0	0	0
98006	19	2.99%	82	4	0	0	0	0	1	0	4	1	0	0	2
98007	3	0.47%	12	0	0	0	3	0	0	0	0	0	0	0	0
98008	4	0.63%	15	0	0	1	0	0	0	0	1	0	0	0	0
98010	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98011	1	0.16%	2	0	0	0	2	0	0	0	1	0	0	0	0
98012	6	0.94%	12	17	0	0	2	0	0	0	1	0	0	0	0
98019	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98020	3	0.47%	15	0	0	0	0	0	0	0	0	0	0	0	0
98021	2	0.31%	10	0	0	0	0	0	0	0	0	0	0	0	0
98022	4	0.63%	20	0	0	0	0	0	0	0	0	0	0	0	0
98023	9	1.42%	33	5	0	0	0	0	0	0	4	0	0	0	0
98025	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98026	4	0.63%	13	0	0	0	4	0	0	0	0	0	0	0	0
98027	12	1.89%	51	0	0	0	0	0	0	0	5	1	0	0	0
98028	4	0.63%	13	4	0	0	0	0	0	0	3	0	0	0	0
98029	13	2.05%	62	0	0	0	0	0	0	0	2	0	0	0	0
98030	12	1.89%	43	9	0	0	3	0	0	0	5	0	0	0	0
98031	8	1.26%	36	0	0	0	0	0	0	0	2	0	0	0	0
98032	8	1.26%	37	0	0	0	0	0	0	0	3	0	0	0	0



98033	11	1.73%	49	0	0	0	0	0	0	0	8	0	0	0	0
98034	10	1.57%	44	0	0	5	0	0	0	0	1	0	0	0	0
98036	6	0.94%	24	4	0	0	0	0	0	0	1	0	0	0	0
98037	2	0.31%	9	0	0	0	0	0	0	0	1	0	0	0	0
98038	20	3.15%	89	4	0	0	0	0	0	0	8	0	0	0	0
98040	6	0.94%	24	0	0	5	0	0	0	0	2	0	0	0	0
98042	28	4.41%	113	5	0	1	0	0	3	0	6	1	0	0	0
98043	1	0.16%	0	0	0	0	0	0	0	0	0	0	0	0	0
98045	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98050	1	0.16%	4	0	0	0	0	0	0	0	0	0	0	0	0
98051	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98052	8	1.26%	31	3	0	0	4	0	0	0	2	0	0	0	2
98053	7	1.10%	29	0	0	0	0	0	0	0	4	0	0	0	0
98055	15	2.36%	63	10	0	0	0	0	0	0	1	0	0	0	0
98056	22	3.46%	94	5	0	0	5	0	0	0	7	0	0	0	0
98057	8	1.26%	39	0	0	0	1	0	1	0	0	0	0	0	0
98058	37	5.83%	146	10	0	3	0	0	2	4	10	1	0	0	1
98059	27	4.25%	120	0	0	1	0	0	0	0	8	0	0	0	0
98065	3	0.47%	8	5	0	0	2	0	0	0	0	0	0	0	0
98070	1	0.16%	0	0	0	0	0	0	0	0	0	0	5	0	0
98072	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98074	9	1.42%	41	0	0	0	0	0	0	0	4	0	0	0	0
98075	10	1.57%	43	0	0	0	0	0	0	0	0	1	0	0	0
98077	2	0.31%	10	0	0	0	0	0	0	0	0	0	0	0	0
98087	3	0.47%	12	0	0	0	1	0	0	0	2	0	0	0	0
98092	27	4.25%	106	8	0	0	0	0	0	1	16	0	0	0	0
98101	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98102	3	0.47%	15	0	0	0	0	0	0	0	0	0	0	0	0
98103	8	1.26%	26	0	7	0	5	0	0	0	2	0	0	0	0
98104	2	0.31%	10	0	0	0	0	0	0	0	0	0	0	0	0
98105	7	1.10%	26	7	0	0	0	0	0	0	1	0	0	0	0
98106	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98107	4	0.63%	15	0	0	0	5	0	0	0	0	0	0	0	0
98109	3	0.47%	14	0	0	0	0	0	0	0	1	0	0	0	0
98110	2	0.31%	0	0	0	0	0	0	0	0	0	0	10	0	0
98112	1	0.16%	2	0	0	0	0	0	3	0	0	0	0	0	0
98115	7	1.10%	29	0	3	0	0	0	0	0	2	0	0	0	0



98116	11	1.73%	54	0	0	0	0	0	0	0	1	1	0	0	0
98117	3	0.47%	10	0	0	0	0	0	0	0	2	0	0	0	0
98118	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98119	5	0.79%	20	4	0	0	0	0	0	0	1	0	0	0	0
98121	1	0.16%	0	0	0	0	5	0	0	0	0	0	0	0	0
98122	6	0.94%	18	9	0	0	0	0	0	0	3	0	0	0	0
98125	3	0.47%	16	0	0	0	0	0	0	0	0	0	0	0	0
98126	6	0.94%	23	0	0	0	5	0	0	0	0	0	0	0	1
98133	2	0.31%	5	0	0	0	3	0	0	0	1	0	0	0	0
98136	3	0.47%	15	0	0	0	0	0	0	0	0	0	0	0	0
98144	3	0.47%	11	2	0	0	2	0	0	0	0	0	0	0	0
98146	4	0.63%	20	0	0	0	0	0	0	0	0	0	0	0	0
98148	7	1.10%	35	0	0	0	0	0	0	0	0	0	0	0	2
98155	1	0.16%	2	2	0	0	0	0	0	0	1	0	0	0	0
98166	5	0.79%	24	0	0	0	0	0	0	0	1	0	0	0	0
98168	2	0.31%	7	0	0	0	0	0	0	0	2	0	0	0	0
98177	1	0.16%	0	5	0	0	0	0	0	0	0	0	0	0	0
98178	5	0.79%	24	0	0	0	0	0	0	0	0	0	0	0	0
98188	3	0.47%	15	0	0	0	0	0	0	0	0	0	0	0	0
98198	9	1.42%	40	0	0	0	0	0	0	0	0	0	0	0	5
98199	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98201	2	0.31%	2	0	0	0	0	1	0	0	2	0	0	0	7
98203	2	0.31%	3	0	0	0	0	4	0	0	2	0	0	0	0
98208	2	0.31%	10	0	0	0	0	0	0	0	0	0	0	0	0
98257	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98258	2	0.31%	5	4	0	0	0	0	0	0	1	0	0	0	0
98264	1	0.16%	0	0	3	0	0	0	0	0	1	0	0	0	0
98270	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98271	1	0.16%	4	0	0	0	0	0	0	0	1	0	0	0	0
98272	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98275	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98296	1	0.16%	0	0	0	0	4	0	0	0	0	0	0	0	1
98311	1	0.16%	2	0	0	0	0	0	0	0	0	0	3	0	0
98321	5	0.79%	20	0	0	1	0	4	0	0	0	0	0	0	0
98329	2	0.31%	9	0	0	0	0	0	0	0	0	0	0	0	0
98332	3	0.47%	6	0	0	0	0	5	0	0	0	0	0	0	0
98335	3	0.47%	14	0	0	0	0	0	0	0	1	0	0	0	0



98338	3	0.47%	10	0	0	0	0	5	0	0	0	0	0	0	0
98359	1	0.16%	0	0	0	0	0	4	0	0	1	0	0	0	0
98360	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98366	4	0.63%	11	0	5	0	0	0	0	0	0	1	3	0	0
98371	3	0.47%	7	0	0	0	0	0	0	0	1	0	0	0	2
98372	4	0.63%	20	0	0	0	0	0	0	0	0	0	0	0	0
98373	1	0.16%	0	5	0	0	0	0	0	0	0	0	0	0	0
98374	9	1.42%	34	4	0	0	0	3	0	0	4	0	0	0	0
98375	3	0.47%	9	0	0	0	0	5	0	0	1	0	0	0	0
98387	4	0.63%	8	0	0	0	0	4	0	0	3	0	0	0	1
98390	2	0.31%	7	0	0	0	0	0	0	0	1	0	0	0	0
98391	10	1.57%	35	4	0	0	0	0	0	0	4	0	0	0	0
98404	2	0.31%	6	5	0	0	0	0	0	0	0	0	0	0	0
98407	3	0.47%	8	0	0	0	0	0	0	0	2	0	0	0	0
98418	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98422	5	0.79%	25	0	0	0	0	0	0	0	0	0	0	0	0
98424	2	0.31%	10	0	0	0	0	0	0	0	0	0	0	0	0
98444	2	0.31%	10	0	0	0	0	0	0	0	0	0	0	0	0
98446	1	0.16%	3	0	0	0	0	0	0	0	4	0	0	0	0
98465	1	0.16%	0	0	0	0	0	0	0	0	0	0	0	0	0
98466	2	0.31%	8	2	0	0	0	0	0	0	0	0	0	0	0
98498	2	0.31%	5	5	0	0	0	0	0	0	0	0	0	0	0
98499	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98502	1	0.16%	0	0	0	4	0	0	0	0	2	0	0	0	0
98513	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98580	1	0.16%	5	0	0	0	0	0	0	0	0	0	0	0	0
98826	1	0.16%	3	0	0	0	0	0	0	0	2	0	0	0	0
98837	1	0.16%	2	0	0	0	0	0	0	0	3	0	0	0	0
98926	1	0.16%	2	0	0	0	0	0	0	0	3	0	0	0	0